



November 30, 2013

TO: Board of Mayor and Aldermen

FROM: Eric Stuckey, City Administrator
Vernon Gerth, Assistant City Administrator for Community & Economic Development
David Parker, CIP Executive/City Engineer
Paul Holzen, Director of Engineering Department
Catherine Powers, Director of Planning & Sustainability Department
Jonathan Langley, Long Range Planning Supervisor of Planning & Sustainability Department

SUBJECT: Draft Plan of Service - Rogers Farm

Purpose

The purpose of this memorandum is to provide the Board of Mayor and Aldermen (BOMA) with information regarding the Annexation and Plan of Service for the Rogers Farm.

Background

Pulte Group has requested annexation of a portion (~400 acres) of the Rogers Farm located within the northwest part of Franklin's Urban Growth Boundary (UGB) and adjoining the current city limits of Franklin. The Pulte Group proposes a 781 detached, single-family dwelling age restricted Del Webb community. The site is located in a rural section of Williamson County and the Plan of Service stipulates the infrastructure the applicant is required to provide to adequately support the project.

Plan of Services

City departments have provided feedback for the Plan of Services and their requirements are outlined in the attached draft. As expected, the Plan of Service primary issues are wastewater and roadway infrastructure. The following is a summary of these improvements:

Wastewater

Based on the West Drainage Basin Study, staff is recommending the following:

- The existing Westhaven Pump Station be relocated to the Rogers Farm and upgraded to handle sewer flow from the entire West Drainage Basin.
- A forty-two inch gravity sanitary sewer interceptor main be constructed to extend the Westhaven Interceptor to the relocated pump station.
- The new pump station will require a sanitary sewer force main to be constructed through the Rogers Farm Development and along Del Rio Pike to the existing fourteen inch (14") sanitary sewer force main in the general vicinity of the intersection of Del Rio Pike and Carlisle Lane and continued along the alignment (paralleling) of the fourteen inch (14") sanitary sewer force main to the City's Water Reclamation Facility on Claude Yates Drive.
- The developer would be responsible for engineering design, necessary easement and right-of-way acquisition, installation of the force main, and paying for construction of the pump station.

Note: In accordance with the City's Municipal Code, recovery is available for wastewater improvements that exceed the infrastructure required for serving the development. It should be expected the sanitary sewer improvements associated with this project will be phased to coincide with development and that developments agreements related to phasing and cost recovery would be presented to the Franklin Municipal Planning Commission and BOMA for consideration concurrently with the Annexation, Rezoning, Development Plan, and Plan of Service.



Street Improvements

The most important aspect of street improvements required to serve this property is the project must have two means of ingress and egress. The proposed secondary means of egress is via existing roads within the Whitehall Subdivision. An alternative would be through the Gentry Farm which would require acquisition and construction of a new road. Other requirements include:

- The applicant has selected access scenario number 1 to assure two means of access. This scenario would require access off Del Rio Pike, south of Whitehall Drive. The second access would be provided by connecting Brinkley Drive and Whitehall Drive within the Whitehall Subdivision. If this scenario cannot be accomplished the City, with Board approval, would need to assist Pulte in condemning easements on the Gentry Farm property to provide the second access.
- Carlisle Lane and the north-south portion of Del Rio Pike are identified in a future major collector. The developer is required to construct a Major Collector section from Highway 96 to the Intersection of Del Rio Pike and Cotton Lane. Specifically, the developer shall provide two twelve-foot drive lanes and a sixteen-foot median, two bicycle lanes, and sidewalks.

Staff has indicated that these improvements are vital to serve this project, even though the improvements will also assist future development.

The street improvements are related primarily to safety for the anticipated dwelling and can be phased, however staff believes the median is essential to the safety concerns and should not be phased. Since this is a collector, recovery is not an option.

Note: There are no off-sets available for the developer for the street improvements outlined. Currently, the City's Road Impact Fee ordinance only authorizes off-sets for improvements made to arterial roads.

Pulte Group – Initial Response to Plan of Service

City staff has met with the representatives from the Pulte Group regarding the draft Pal of Service. While there is agreement to the phasing of improvements to coincide with development, Pulte Group does not agree to the street improvements proposed by staff and have offered an alternative. Attached is a Plan of Service that includes changes (highlighted in red) as proposed by the Pulte Group.

Financial Impact

Included separately, staff has prepared a rough estimate of the revenues likely to be generated from the development of this property. This estimate is based on 800 homes with an average value of \$250,000 and does not include an estimate of the annual cost of the city providing service to this property when fully developed.

Recommendation

As discussed, a Public Hearing has been scheduled during the December 10th BOMA meeting to obtain additional input from citizens, the applicant, and staff. Following the public hearing, staff recommends one of the following motions:

Option I:

A motion not to proceed with a Plan of Service or annexation of the Rogers Farm as requested at this time, or;

Option II:

Motion to refer the Plan of Service to staff (with or without comments) to begin working with the applicant on the Annexation, Rezoning, Development Plan, Plan of Service, and related development agreements. It should be noted that direction by the Board to refer the Plan of Service into the annexation process does not imply approval by the Board during later stages in the process. The referral simply moves the item for further work and review by staff, the FMPC and, ultimately, the Board.



City of Franklin Land Use Plan

The Rogers Farm is located within the West Harpeth Character Area - Special Area 3. The following is an excerpt from the City of Franklin Land Use Plan which supports residential development in this area.

WESTHARPETH PLANNING BACKGROUND

The West Harpeth Character Area contains 6,120 acres located on the northwestern edge of the UGB. New Highway 96 West transverses the area, and Hillsboro Road is near the eastern boundary. The area is bound to the northeast by the Harpeth River and to the west and northwest by the West Harpeth River. The Central Franklin Character Area is to the east, and the Southall Character Area bounds the area to the south.

About one-half of the land in the area is vacant or agricultural. The developed land is primarily residential, although nonresidential uses have been approved in conjunction with the Westhaven development. The Harpeth and West Harpeth Rivers create substantial floodplain constraints. The area is also home to historic farmstead sites.

This area is in the midst of rapid change. Major planned developments are in the process of approval and construction. The Westhaven TND is the dominant new development for the area. There will be some 2,700 dwelling units provided by this and other approved developments. These developments are taking place in comprehensive mixed-use designs, with a variety of housing types integrated with Neighborhood and Local Retail, office spaces and community open space and facilities.

The forces that may have the greatest impact on the future character of the West Harpeth Character Area include the improvement of infrastructure. Both the completion of Mack Hatcher Parkway and expanded power lines will open the area to additional development. A sewer line is being constructed to handle the future needs of the area.

While property rights are respected, new development should be timed with availability of infrastructure. The subdivision of land into one-acre lots proliferate sprawl and is inappropriate in this area.

Vision

The West Harpeth area will be an exemplary model for the way in which new development can be accommodated while preserving open space, sensitive environmental features and historic farmsteads. It will be an area of high quality guided by design standards and mixed-use developments.

Special Area 3

1. This area is appropriate for a Hamlet, a Conservation Subdivision or a TND pursuant to a Master Plan and that at least fifty percent of the area—targeted towards environmentally sensitive or scenic areas and vistas—is preserved as permanent open space.
2. A 300-foot setback is recommended for the north side of New Highway 96 West.
3. Commercial, Office and Retail uses shall not front along New Highway 96 West.

RESOLUTION 2013-XX

A RESOLUTION, AS AMENDED, ADOPTING A PLAN OF SERVICE FOR THE ANNEXATION OF CERTAIN AREAS BY THE CITY OF FRANKLIN, TENNESSEE

WHEREAS, Tennessee Code Annotated (TCA) 6-51-102 as amended requires that a Plan of Service be adopted by the governing body of a City prior to the passage of an annexation ordinance.

DEL RIO PIKE AREA

The proposed annexation area is located along Del Rio Pike with frontage on portions of its northern and eastern boundary and also along the West Harpeth River on the western border of the site. The site address associated with this property is 3021 Del Rio Pike (Map 51, Parcel 13). A map is attached as Exhibit A. The area comprises a total of 400 acres, in 1 parcel, with 1 owner. The owners have requested annexation by the City of Franklin. The area currently is vacant.

WHEREAS, it is now believed in the best interest of the City of Franklin to annex such area to provide for future development in accordance with City standards,

NOW, THEREFORE, BE IT RESOLVED BY THE CITY OF FRANKLIN, TENNESSEE, AS FOLLOWS:

SECTION 1. Pursuant to the provisions of TCA section 6-51-102, there is hereby adopted, for the area bounded as described above, the following Plan of Service:

A. Water

Water for domestic, commercial, and industrial use shall be provided at prevailing City rates, from existing City mains, on the effective date of annexation.

Water for fire protection of the vacant property shall be available on the effective date of annexation by means of two (2) existing fire hydrants located approximately three quarters (3/4) of a mile from the property. The developer shall be responsible for the extension of water mains as development occurs to maintain minimum fire protection capabilities in accordance with the International Fire Code (latest revision). Should booster stations be required to maintain the minimum flow and/or pressure, the developer shall be required to provide the engineering design and construction of such facilities.

The City of Franklin has a twenty-four inch (24") water transmission main that currently runs along Del Rio Pike and is adjacent to the property. The developer shall be responsible for the engineering design and construction

required to connect to this main and for the water mains, fire hydrants, and other required appurtenances necessary to serve the property.

B. Reclaimed Water

The developer of the property shall be required to extend reclaimed water infrastructure to the site and provide the infrastructure to serve individual lots (customers) per current City policies and standards. Based on current policies, the service is available and the applicant is responsible for extending and providing the appropriate infrastructure on the property.

C. Wastewater

This property lies within the West Drainage Basin as defined in the City of Franklin Sanitary Sewer Basin Study documents. The City completed a sanitary sewer basin study update for this area in March of 2013, and BOMA endorsed Option 1 (Exhibit B-1) from that study in City of Franklin Resolution No. 2013-19. Option 1 (Exhibit B-1) is attached as Exhibit B. Option 1 calls for the existing Westhaven Pump Station to be relocated to the Rogers Farm and upsized to be able to handle the anticipated sewage flows from the entire West Basin. In addition, a new forty-two inch (42") gravity sanitary sewer interceptor main will have to be constructed to extend the Westhaven Interceptor to the new relocated pump station. The new pump station will require a sanitary sewer force main to be constructed through the Rogers Farm Development and along Del Rio Pike to the existing fourteen inch (14") sanitary sewer force main in the general vicinity of the intersection of Del Rio Pike and Carlisle Lane and continued along the alignment (paralleling) of the fourteen inch (14") sanitary sewer force main to the City's Water Reclamation Facility on Claude Yates Drive. The developer shall be responsible for the engineering design utilizing the design firm as selected by the City, construction, and all necessary easements, rights-of-way (ROW), and/or fee simple land dedication for the sanitary sewer gravity mains, force mains, and pump stations (whether on-site or off-site) required for the build-out of the infrastructure as shown in the West Basin Sanitary Sewer Study (latest revision) related to the Westhaven Pump Station relocation and integral sanitary gravity interceptor and force main.

It should be noted that the West Sanitary Sewer Basin Study indicates that the existing fourteen inch (14") sanitary sewer force main that conveys sewage from the Westhaven Pump Station to the City's Water Reclamation Facility will require upsizing or paralleling when the West Basin is thirty-five and five tenths percent (35.5%) developed. The anticipated peak flow contributable to this amount of development is roughly equivalent to five million four hundred thousand gallons per day (5.4 MGD). The upsizing of this force main is exclusive of the contribution of sewage flows from the Southwest No. 1 Drainage Basin as defined in the West Sanitary Sewer Basin Study.

The proposed development is anticipated to construct approximately eight hundred (800) single family residential homes. The developer(s) shall be responsible for the engineering design, construction and all necessary

easements and rights-of-way (ROW) for the sanitary sewer infrastructure required to serve the development of this property through the relocated Westhaven Pump Station.

D. Stormwater

The developer shall be responsible for the engineering, design and construction of the necessary Stormwater infrastructure for the development of this property and shall comply with all requirements as outlined in the Franklin Municipal Code. As the property develops, the City of Franklin will charge prevailing stormwater user fees per policies and procedures as outline in the Franklin Municipal Code.

E. Streets

The applicant requesting annexation had a traffic study prepared and submitted to the City prior to the annexation request. The study was completed in August 2013, and updated with an addendum in September 2013. The study identified three (3) different access scenarios and the applicant has selected Access Scenario 1 as their preferred option. The Developer is required to provide two (2) ways of ingress and egress per City standards to access the property. Any changes to Access Scenario 1 shall require additional conditions of approval or a revised plan of service prior to development plan approval. The following requirements are prepared in reference to the City of Franklin Street Standards, Zoning Ordinance, and the Major Thoroughfare Plan (MTP).

The North West area of the City and Urban Growth Boundary is largely rural with substandard roadways. The rural roadways in this area are generally described as being in poor condition and will not be adequate as this area continues to transition into an urban environment. Pavements in many areas show signs of rutting, oxidation, alligator cracking and subgrade failure. Also, the roadway geometry and alignment are substandard based on the City's Transportation & Street Technical Standards. In light of this context, City staff recommends that the developer of the property be required to provide the following improvements to the City's street network. Exhibit C, as provided below, identifies the MTP routes for the North West area of the City.

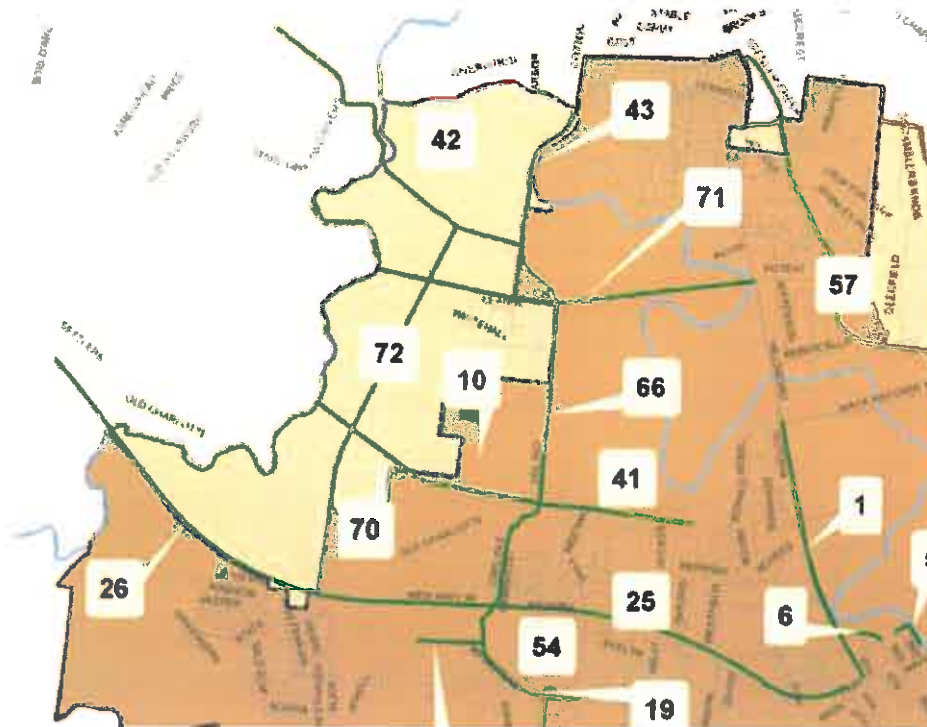


Exhibit C

Primary access will be provided for the property at one location on the north-south portion of Del Rio Pike, south of Whitehall Drive. Two secondary accesses shall be provided by connecting to Brinkley Drive and Whitehall Drive within the Whitehall subdivision. The transportation network shall be upgraded as follows.

Carlisle Lane and the north-south portion of Del Rio Pike are currently identified as a future Major Collector roadways. The developer shall be required to construct a Major Collector section from Highway 96 to the Intersection of Del Rio Pike and Cotton Lane as shown in Exhibit D, as provided below. The developer is specifically responsible for addressing horizontal geometric deficiencies as well as providing two (2) 12-foot travel lanes separated by a sixteen foot (16') median to provide access control, two (2) 4-foot bicycle lanes, a sidewalk on both sides of the roadway, roundabout junctions (or turn lanes as necessary), a curb and gutter drainage system and all necessary rights-of-way and easements. The developer shall be responsible for the removal and restoration of roadways that are abandoned as a result of these improvements. In addition, the improvements shall be coordinated with the Mack Hatcher NW Extension Project for the proposed intersection of Mack Hatcher and Del Rio Pike. The estimated cost of these improvements is \$5.6 million.

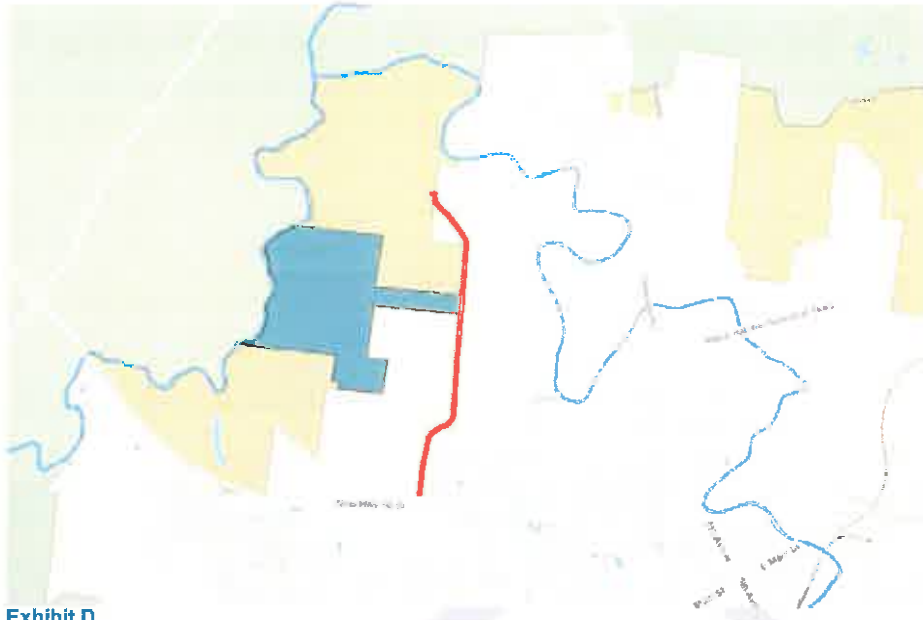


Exhibit D

The east-west portion of Del Rio Pike is currently identified as a future Major Collector roadway. The developer shall be required to construct a Major Collector section from the intersection with Carlisle Lane to the existing improved section along Del Rio Pike as shown in Exhibit E, as provided below. The developer is specifically responsible for providing two (2) 12-foot travel lanes, two (2) 4-foot bicycle lanes, sidewalks on both side of the roadway, roundabout junctions (or turn lanes as necessary), a curb and gutter drainage system and all necessary rights-of-way and easements. The estimated cost of this improvement is \$3.5 million dollars.

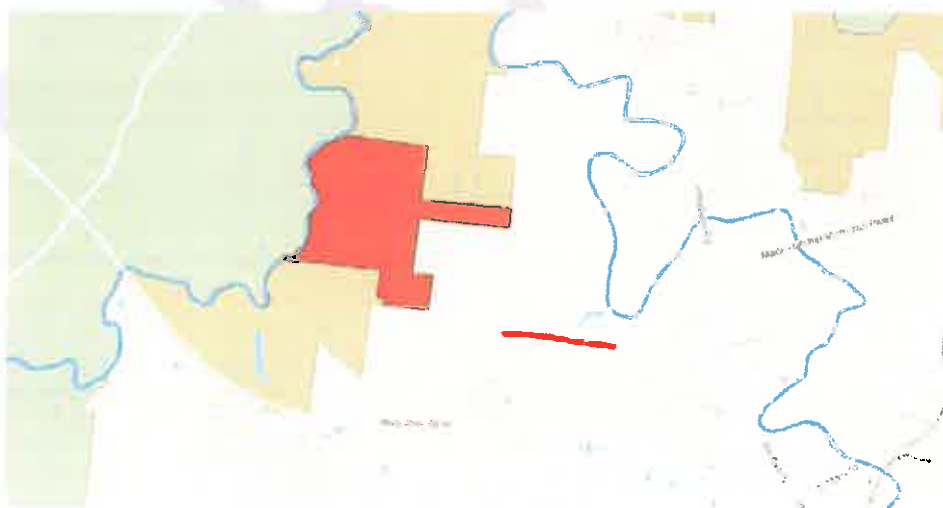


Exhibit C

The Developer shall provide preliminary roadway alignment studies for projects 70, 71 and 72 as shown in Exhibit C and included in the Major Thoroughfare Plan. In addition, the developer shall be required to dedicate all necessary rights-of-way for these improvements, rough grading to accommodate the future improvements of project 71 and

project 70 and final construction for project 72 across the proposed development.

E. Traffic Control

Traffic signals, traffic signs, street markings, and other traffic control devices shall be installed in accordance with the Manual on Uniform Traffic Control Devices (MUTCD) for Streets and Highways (latest revision) as the need is established by appropriate study and traffic standards. No needs have been identified for the area at this time. New development after the effective date of annexation shall comply with City regulations governing traffic markings.

F. Planning and Zoning

The planning and zoning jurisdiction of the City will extend to the annexed area on the effective date of annexation. City planning will thereafter encompass the annexed area.

G. Street Lighting

Off-site street lighting shall be identified and installed concurrently with required off-site roadway improvements and shall be funded and installed by the developer per City policy. Off-site street lighting shall include the installation of electric metering to allow for City ownership and maintenance. Street lighting within new developments shall be funded and installed by the developer per City policy, which includes the installation of electric meters to allow for future ownership and maintenance by the City or homeowners association. The installation and operation of street lights shall coincide with the phasing of development and issuance of Certificates of Occupancy.

H. Parks and Recreation

According to the existing parkland dedication ordinance, the development would pay fees, dedicate land, or submit a Modification of Standard (MOS) for easements related to construction of a greenway along the Harpeth River section, trails within the property and/or canoe launch along the river frontage.

788 units break down $3.5 \times 1200 \text{sf} = 42,000 \text{sf} + 753 \times 600 \text{sf} = 451,800 \text{sf}$
 $42,000 \text{sf} + 451,800 \text{sf} = 493,800 \text{sf}$ or 11.336 + acres needed to meet parkland dedication requirements.

5.5.4 Dedication of Public Land for Parks and Greenways/Blueways In addition to the standards for open space set-asides, residential or mixed-use developments with ten (10) or more dwelling units shall dedicate land to the city for use as public parks or greenways/blueways in accordance with the following standards. For a revision to a PUD approved prior to July 1, 2008, the Public Land Dedication shall only be assessed on any of the new residential dwelling units to be included within the PUD.

(1) Amount to be Dedicated

- (a) Developments required to dedicate land shall do so in the following amounts:
 - (i) 1,200 square feet per dwelling unit for the first 35 principal dwelling units; and
 - (ii) 600 square feet for each additional principal dwelling unit beyond the first 35.
- (b) Accessory dwelling units shall be exempt from these requirements.
- (c) Development proposed in **phases** shall be considered as a single development for the purposed of applying the land dedication standards. Development shall not be phases to avoid the requirements of this subsection.

I. Street Name Signs

No additional street name signs in the area have been deemed necessary. New development shall install street name signs per City policy. Street name signs at the intersections of Major Collector roadways shall be LED (light emitting diode) internally illuminated in accordance with the Franklin Transportation & Street Technical Standards specifications.

J. Fire Protection

Fire protection by the present personnel and equipment of the Franklin Fire Department will be provided on and after the effective date of annexation. The proposed annexation area is currently within the Fire Station No. 1 primary response area. No additional manpower or equipment will be necessary to serve the annexation area at this time.

K. Police Protection

Police patrol and response to calls by the Franklin Police Department will be provided on and after the effective date of annexation. Police services will be provided with existing personnel and equipment.

L. Building and Code Inspection Services

Any Building and Code inspection services now provided by the City will begin in the annexed area on the effective date of annexation. New development after the effective date of annexation shall comply with City building codes. New development shall pay all applicable fees and permits for inspection services.

SECTION 2. This Resolution shall be effective from and after its adoption.

ADOPTED THIS _____.

ERIC S. STUCKEY
City Administrator

DR. KEN MOORE
Mayor

RESOLUTION 2013-__

A RESOLUTION, AS AMENDED, ADOPTING A PLAN OF SERVICE FOR THE ANNEXATION OF CERTAIN AREAS BY THE CITY OF FRANKLIN, TENNESSEE

WHEREAS, Tennessee Code Annotated (TCA) 6-51-102 as amended requires that a Plan of Service be adopted by the governing body of a City prior to the passage of an annexation ordinance.

DEL RIO PIKE AREA

The proposed annexation area is located along Del Rio Pike with frontage on a portions of its ~~northern and~~ eastern boundary. ~~The western border of the site is comprised of the and also along the~~ West Harpeth River ~~on the western border of the site~~. The site address associated with this property is 3021 Del Rio Pike (Map 51, Parcel 13). A map is attached as Exhibit A. The area comprises a total of 400 acres, in 1 parcel, with 1 owner. The owners ~~have~~ has requested annexation by the City of Franklin. The area currently is vacant.

WHEREAS, it is now believed in the best interest of the City of Franklin to annex such area to provide for future development in accordance with City standards,

NOW, THEREFORE, BE IT RESOLVED BY THE CITY OF FRANKLIN, TENNESSEE, AS FOLLOWS:

SECTION 1. Pursuant to the provisions of TCA section 6-51-102, there is hereby adopted, for the area bounded as described above, the following Plan of Service:

A. Water

Water for domestic, commercial, and industrial use shall be provided at prevailing City rates, from existing City mains, on the effective date of annexation.

Water for fire protection of the vacant property shall be available on the effective date of annexation by means of two (2) existing fire hydrants located approximately three quarters (3/4) of a mile from the property. The developer shall be responsible for the extension of water mains as development occurs to maintain minimum fire protection capabilities in accordance with the International Fire Code (latest revision). Should booster stations be required to maintain the minimum flow and/or pressure, the developer shall be required to provide the engineering design and construction of such facilities.

The City of Franklin has a twenty-four inch (24") water transmission main that currently runs along Del Rio Pike and is adjacent to the property. The developer shall be responsible for the engineering design and construction

required to connect to this main and for the water mains, fire hydrants, and other required appurtenances necessary to serve the property.

B. Reclaimed Water

~~The City's current policies require that new developments provide for the distribution of reclaimed water to be used for non-potable irrigation purposes. Under City policies and in order to provide the property with access to reclaimed water a reclaimed water line would need to be extended by the City of Franklin from existing location near Del Rio and Carlisle Lane to within 1,000 feet of the proposed annexation. As the Plan of Service is more fully developed, the feasibility of the aforementioned extension and a further definition of the planned eventual use of reclaimed water within the proposed annexation will be determined. Infrastructure deemed necessary to distribute reclaimed water within the annexed area will be the responsibility of the developer. The developer of the property shall be required to extend reclaimed water infrastructure to the site and provide the infrastructure to serve individual lots (customers) per current City policies and standards. Based on current policies, the service is available and the applicant is responsible for extending and providing the appropriate infrastructure on the property.~~

C. Wastewater

~~The is-proposed erty annexation area lies within the West Drainage Basin as defined in the City of Franklin Sanitary Sewer Basin Study documents. The West Drainage Basin includes existing developed and developing areas, undeveloped areas within the current City limits (primarily the Moore Property) and the undeveloped areas within the City's northwest Urban Growth Boundary. The City completed ~~a~~ sanitary sewer basin study update for this area in March of 2013, and BOMA endorsed Option 1 (Exhibit B-1) from that study in City of Franklin Resolution No. 2013-19. Option 1 (Exhibit B-1) is attached as Exhibit A.~~

~~Option 1 calls for the existing Westhaven Pump Station to be relocated to replaced with a new proposed West Basin Primary Pump Station (the WBPPS) the within the Rogers Farm proposed annexation and to ultimately be upsized to be able to handle design capacities needed for the anticipated sewage flows from the entire West Basin. The WBPPS is expected to be necessary when the sewage flows entering the existing Westhaven Pump Station reach the design capacity of the facility. When the new facility is constructed, in addition, a new forty-two inch (42") gravity sanitary sewer interceptor main will have to be constructed to extend the Westhaven Interceptor through portions of the lands within the current Urban Growth Boundary (the Gentry Farm) to the new relocated pump station WBPPS location. A sanitary sewer force main will be constructed with The new pump station will require a sanitary sewer force main to be constructed through the Rogers Farm Development and along Del Rio Pike to make the connection with the existing Westhaven Pump Station fourteen inch (14") sanitary sewer force main. This existing force main is located in the general vicinity of the intersection of Old Charlotte Pike and Del Rio Pike and Carlisle Lane and continued along east along Del Rio~~

~~Pike terminating at the alignment (paralleling) of the fourteen-inch (14") sanitary sewer force main to the City's Water Reclamation Facility on Claude Yates Drive. It should be noted that the West Sanitary Sewer Basin Study indicates that the existing fourteen inch (14") sanitary sewer force main that conveys sewage from the Westhaven Pump Station to the City's Water Reclamation Facility will require upsizing or paralleling when the West Basin is 35.5% developed. The anticipated peak flow contributable to this amount of development is roughly equivalent to five million four hundred thousand gallons per day (5.4 MGD).~~

~~The design of the sanitary sewer facilities to be installed by the developer that will be needed for the build-out of the proposed annexation will take into consideration the infrastructure shown in the West Basin Sanitary Sewer Study (latest revision). The developer shall be responsible for the engineering design of these facilities utilizing the a design firm as selected approved by the City. The design, construction, and all necessary easements, rights-of-way (ROW), and/or fee simple land dedications for the sanitary sewer gravity mains, force mains, and pump stations (whether on-site or off-site) will 1) be sized to accommodate the service to the annexed property and 2) allow for future incremental construction of additional capacity by others to accommodate the West Basin Sanitary Sewer System as the growth of the City occurs within the basin beyond the annexed area, required for the build-out of the infrastructure as shown in the West Basin Sanitary Sewer Study (latest revision) related to the Westhaven Pump Station relocation and integral sanitary gravity interceptor and force main.~~

~~It should be noted that the West Sanitary Sewer Basin Study indicates that the existing fourteen-inch (14") sanitary sewer force main that conveys sewage from the Westhaven Pump Station to the City's Water Reclamation Facility will require upsizing or paralleling when the West Basin is thirty-five and five tenths percent (35.5%) developed. The anticipated peak flow contributable to this amount of development is roughly equivalent to five million four hundred thousand gallons per day (5.4 MGD). The upsizing of this force main is exclusive of the contribution of sewage flows from the Southwest No. 1 Drainage Basin as defined in the West Sanitary Sewer Basin Study.~~

~~The proposed development is anticipated to construct approximately eight hundred (800) single-family residential homes. The developer(s) shall be responsible for the engineering design, construction all necessary easements and rights-of-way (ROW) for the sanitary sewer infrastructure required serve the development of this property through the relocated Westhaven Pump Station.~~

D. Stormwater

The developer shall be responsible for the engineering, design and construction of the necessary Stormwater infrastructure for the development of this property and shall comply with all requirements as

outlined in the Franklin Municipal Code. As the property develops, the City of Franklin will charge prevailing stormwater user fees per policies and procedures as outline in the Franklin Municipal Code.

E. Streets

~~The applicant requesting annexation had a traffic study prepared and submitted to the City prior to the annexation request. The study was completed in August 2013, and updated with an addendum in September 2013. The study identified three different access scenarios and the applicant has selected Access Scenario 1 as their preferred option. The Developer is required to provide two ways of ingress and egress per City standards to access the property. Any changes to Access Scenario 1 shall require additional conditions of approval or a revised plan of service prior to development plan approval. The following requirements are prepared in reference to the City of Franklin Street Standards, Zoning Ordinance, and the Major Thoroughfare Plan (MTP).~~

~~Under existing conditions the transportation linkages within the The North West area of the City and Urban Growth Boundary is are largely two lane rural roadway corridors absent of traditional urban features such as sidewalks, bike lanes and curbs with substandard roadways. The rural roadways in this the area are generally described as being in poor conditions and are deemed inadequate for existing and future transportation needs. will not be adequate as As this area continues to transition into an urban environment the city will be accommodating the transition by implementing the roadway network contained in the City's Major Thoroughfare Plan. Pavements in many areas show signs of rutting, oxidation, alligator cracking and subgrade failure. Also, the roadway geometry and alignment are substandard based on the City's Transportation & Street Technical Standards.~~

~~It is important to note that the projects recommended in the City's Major Thoroughfare Plan were identified in the context of the construction of Mack Hatcher Blvd. from Hillsboro Road to HWY 96. The City has spent considerable time and effort to promote the development of the northwest quadrant of Mack Hatcher Blvd. Prior City of Franklin commitments to this important roadway include the acquisition of all necessary rights of way and the engineering design of the facility. The Mack Hatcher improvement was to be included in the TDOT capital improvement program however in light of TDOT funding limitations the road was removed from the state's recent improvement program and the project is now well down the state's list of transportation improvement priorities such that the timing of the eventual roadway construction is not expected to occur as soon as desired.~~

~~Transportation studies have been provided by the developer to assess the added vehicular volumes that the development of the proposed annexation would have on area roadways. These studies have been reviewed by the City Staff and found to be acceptable. The underlying assumptions included in those studies assume that Westhaven and other~~

currently approved projects are fully built out as currently planned and that the proposed annexation is fully built out as an age restricted (55yrs and older) active adult community. The traffic generation factors used in transportation studies for senior adult single family residential development indicate (and have been validated to confirm) that an age restricted community produces less than 40% of the traffic volume of a similarly sized traditional neighborhood containing single family detached homes. The traffic study results indicated that the existing roadways have the capacity to convey the predicted traffic volumes with acceptable levels of service.

In light of ~~this~~ the context outlined above, (i.e. that the existing roadways have sufficient capacity) ~~the~~ City staff recommends that the developer of the property be required to provide ~~the following~~ improvements to the City's street network to enhance the safety of the roadways. Exhibit A, as attached, identifies the MTP routes for the North West area of the City.

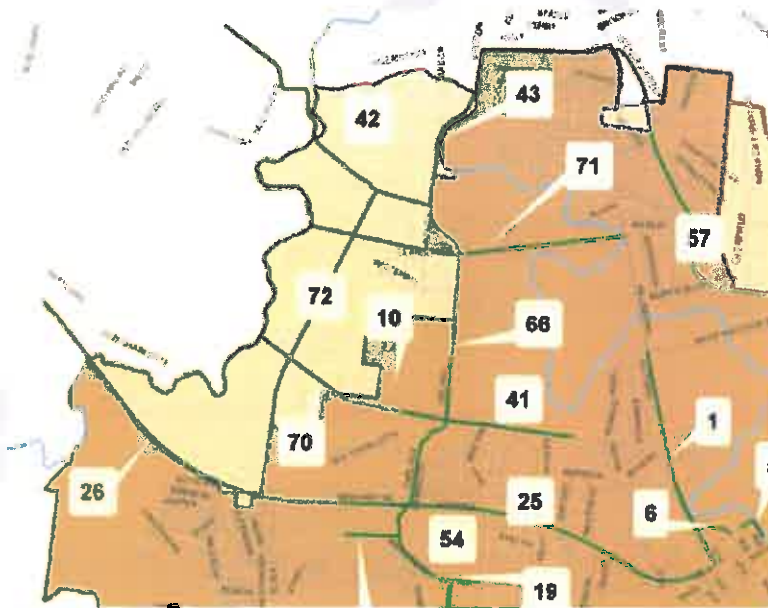


Exhibit A

Primary access will be provided for the property at one location on the north-south portion of Del Rio Pike, south of Whitehall Drive. Two secondary accesses ~~shall will~~ be provided by connecting to Brinkley Drive and Whitehall Dr. within at the boundary of the Whitehall subdivision. ~~The transportation network shall be upgraded as follows:~~

The following projects are recommended to be implemented as phased improvements being made in conjunction with the development of the annexed area. The timing of the phasing will determined through further consideration of the plan for the annexed area as it is reviewed and approved by the City.

Phase 1

The developer will construct 1) a portion of COF MTP Project 66 consisting of Carlisle Lane from HWY 96 to and the north-south portion of Del Rio Pike and 2) a proposed left turn lane on Del Rio Pike at the primary entrance to the annexed property. The proposed Carlisle improvements will be constructed with a modified section of COF TS-9 that includes two 12 ft travel lanes, two 4 ft bike lanes and curb and gutter. The remainder of the section's improvements (i.e. sidewalks) will occur with the future development of the lands adjacent to the roadway, are currently identified as a future major collector roadway. The developer shall be required to construct a Major collector section from Highway 96 to the Intersection of Del Rio Pike and Cotton Ln as shown in Exhibit B. The developer is specifically responsible for addressing horizontal geometric deficiencies as well as providing two 12-foot travel lanes separated by a 16' median to provide access control, two 4-foot bicycle lanes, a sidewalk on both sides of the roadway, roundabout junctions (or turn lanes as necessary), a curb and gutter drainage system and all necessary right-of-way and easements. The Developer shall be responsible for the removal and restoration of roadways that are abandoned as a result of these improvements. In addition the improvements shall be coordinated with the Mack Hatcher NW Extension Project for the proposed intersection of Mack Hatcher and Del Rio Pike. The estimated cost of these improvements is \$5.6 million.

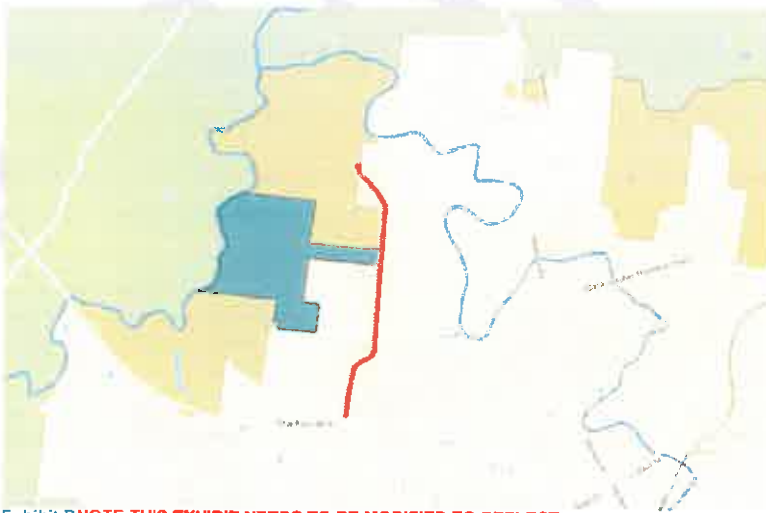


Exhibit B NOTE-THIS EXHIBIT NEEDS TO BE MODIFIED TO REFLECT THE DESCRIPTION OF IMPROVEMENTS LISTED ABOVE

Phase 2

The developer will construct a portion of COF MTP Project 41 consisting of Del Rio Pike from Poplar Grove Elementary School to Carlisle Lane. The proposed Del Rio Pike improvements will be constructed as a modified section of COF TS-9 that includes two 12 ft travel lanes, two 4 ft bike lanes and curb and gutter. The remainder of the section's

improvements (i.e. sidewalks) will occur with the future development of the lands adjacent to the roadway.

The east-west portion of Del Rio Pike is currently identified as a future major collector roadway. The developer shall be required to construct a major collector section from the intersection with Carlisle Lane to the existing improved section along Del Rio Pike as shown in Exhibit C. The developer is specifically responsible for providing two 12-foot travel lanes, two 4-foot bicycle lanes, sidewalks on both side of the roadway, roundabout junctions (or turn lanes as necessary), a curb and gutter drainage system and all necessary right-of-way and easements. The estimated cost of this improvement is \$3.5 million dollars.

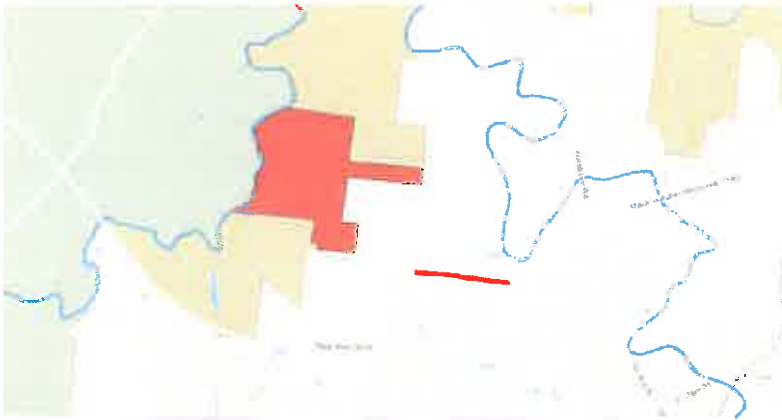


Exhibit C

Phase 3

The developer will construct a portion of COF MTP Project 66 consisting of Del Rio Pike from Carlisle Lane to the primary entrance to the annexed property. The proposed Del Rio Pike improvements will be constructed as a modified section of COF TS-9 that includes two 12 ft travel lanes, two 4 ft bike lanes and curb and gutter. The remainder of the section's improvements (i.e. sidewalks) will occur with the future development of the lands adjacent to the roadway.

COF MTP Project 72

The development of the annexed area will include the construction of a significant portion of COF MTP Project 72 within its boundaries. The design of the segment of Project 72 (a minor collector per COF std. dwg. no. TS-9) that is within the boundaries of the proposed annexation will include the study of future Project 70 and 71 alignments. These future roadways (anticipated to be constructed by others) are located outside but adjacent to the annexed property and are being studied to confirm that the future horizontal and vertical alignments of the projects can be accommodated. The Developer shall provide preliminary roadway alignment study for projects 70, 71 and 72 as shown in Exhibit A and included in the Major Thoroughfare Plan. In addition the developer shall be required to dedicate all necessary right-of-way for these improvements,

~~rough grading to accommodate the future improvements of project 71 and project 70 and final construction for project 72 across the proposed development.~~

E. Right of Way Acquisition

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Rights of way and easements necessary to fulfill the plan of service will be acquired by the COF using its condemnation powers (when necessary) with the developer funding the costs incurred during the process.

EF. Traffic Control

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Traffic signals, traffic signs, street markings, and other traffic control devices will be installed as the need is established by appropriate study and traffic standards. No needs have been identified for the area at this time. New development after the effective date of annexation shall comply with City regulations governing traffic markings.

FG. Planning and Zoning

The planning and zoning jurisdiction of the City will extend to the annexed area on the effective date of annexation. City planning will thereafter encompass the annexed area.

GH. Street Lighting

Off-site street lighting shall be identified and installed at intersections concurrently with required off-site roadway with improvements and shall be funded and installed by the developer per City policy which may include the installation of electric meters to allow for future ownership and maintenance by the City in accordance with City policy. Street lighting within new developments shall be funded and installed by the developer per City policy which includes the installation of electric meters to allow for future ownership and maintenance by the City or homeowners association. The installation and operation of street lights shall coincide with phasing of development and issuance of Certificates of Occupancy.

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HI. Parks and Recreation

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According to the existing parkland dedication ordinance, the development would pay fees, dedicated land or submit a Modification of Standard (MOS) for easements related to construction of a greenway along the Harpeth River section, trails within the property and/or canoe launch the river frontage.

788 units break down 3.5x1200sf=42,000sf+753x600sf=451,800sf
42,000sf+451,800sf=493,800sf or 11.336 + acres needed to meet
parkland dedication requirements.

5.5.4 Dedication of Public Land for Parks and Greenways/Blueways In addition to the standards for open space set-asides, residential or mixed-use developments with ten (10) or more dwelling units shall dedicate land to the city for use as public parks or greenways/blueways in accordance with the following standards. For a revision to a PUD approved prior to

July 1, 2008, the Public Land Dedication shall only be assessed on a ny of the new residential dwelling units to be included within the PUD.

- (1) Amount to be Dedicated
 - (a) Developments required to dedicate land shall do so in the following amounts:
 - (i) 1,200 square feet per dwelling unit for the first 35 principal dwelling units; and
 - (ii) 600 square feet for each additional principal dwelling unit beyond the first 35.
 - (b) Accessory dwelling units shall be exempt from these requirements.
 - (c) Development proposed in phases shall be considered as a single development for the purposed of applying the land dedication standards. Development shall not be phases to avoid the requirements of this subsection.

IJ. **Street Name Signs**

No additional street name signs in the area have been deemed necessary. New development shall install street name signs per City policy.

JK. **Fire Protection**

Fire protection by the present personnel and equipment of the Franklin Fire Department will be provided on and after the effective date of annexation. The proposed annexation area is currently within the Fire Station No. 1 primary response area. No additional manpower or equipment will be necessary to serve the annexation area at this time.

KL. **Police Protection**

Police patrol and response to calls by the Franklin Police Department will be provided on and after the effective date of annexation. Police services will be provided with existing personnel and equipment.

LM. **Building and Code Inspection Services**

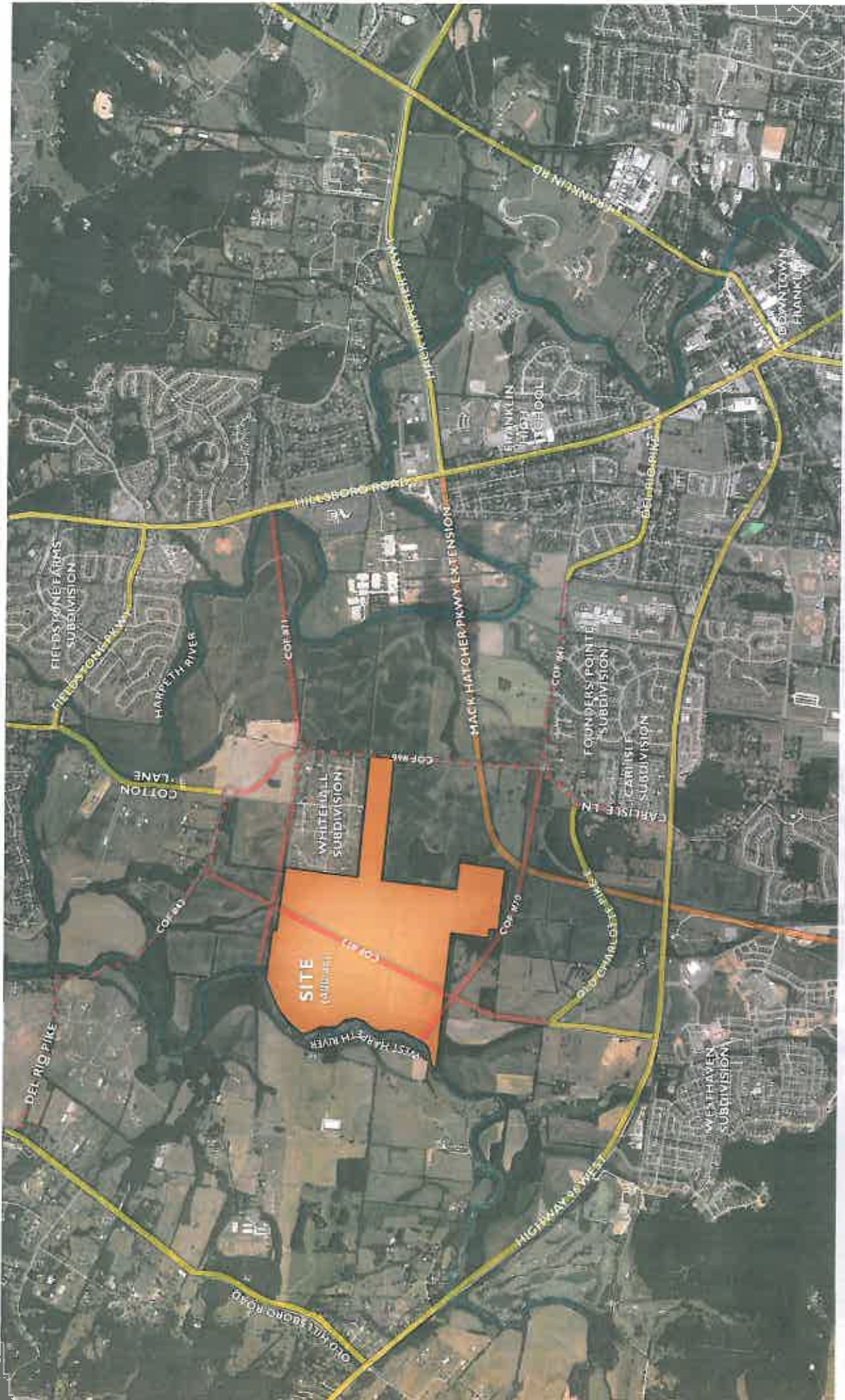
Any Building and Code inspection services now provided by the City will begin in the annexed area on the effective date of annexation. New development after the effective date of annexation shall comply with City building codes. New development shall pay all applicable fees and permits for inspection services.

SECTION 2. This Resolution shall be effective from and after its adoption.

ADOPTED THIS _____.

ERIC S. STUCKEY
City Administrator

DR. KEN MOORE
Mayor



Project # 2006
October 26, 2013

VICINITY MAP
ROGERS FARM PROPERTY
 City of Franklin, Williamson County, Tennessee

CAST VOORIN DESIGN
 SITEWORK ENGINEERING ARCHITECTS

Del Webb

**CITY of FRANKLIN
 POTENTIAL DEVELOPMENT SITE (ROGERS SITE) REVENUES**

ASSUMPTIONS

800 SINGLE FAMILY HOMES
 1,500 SQUARE FEET EACH
 \$250,000 VALUE PER RESIDENCE
 DOES NOT PROVIDE FOR OFFSETS/RECOVERIES
 NO GOLF COURSE (i.e. minimum/no impact for recreational facilities)
 HOUSEHOLD SIZE IS 1.5

ALL FEES USE CURRENT PRICING
 ALL DEVELOPED AT THE SAME TIME
 ALL FEES PAID AT THE SAME TIME
 RECLAIMED WATER AVAILABLE
 NO IRRIGATION WATER SINCE RECLAIMED AVAILABLE

PROJECTED REVENUES

ROAD IMPACT FEE	\$4,227/unit	\$	3,381,600
FACILITIES TAX	\$0.89/ft ²	\$	1,068,000
BUILDING PERMITS/LICENSES	\$800/unit	\$	640,000
TRASH CANS	\$75/unit	\$	60,000
WATER CONNECTION FEES	\$2,429/unit	\$	1,943,200
SEWER CONNECTION FEES	\$4,257/unit	\$	3,405,600
RECLAIMED WATER CONNECTION FEES (assume 25% tie on)	\$1,220/unit	\$	244,000
TOTAL - ONE TIME DEVELOPMENT FEES		\$	10,742,400
PROPERTY TAX	\$0.3765 tax rate	\$	188,250
WATER USER FEE (Reid Hill average in 2012 was 1,873 gallons/month)	2,000 gallons/month	\$	143,808
SEWER USER FEE (Reid Hill average)	2,000 gallons/month	\$	186,624
RECLAIMED USER FEE (Cornerstone HOA avg = 750k/mo & Resid is 9k/mo)	9,000 gals/mo (Res) 750k gal/mo HOA	\$	30,000
TRASH USER FEE	\$15/month	\$	144,000
STORMWATER USER FEE	Resid small (\$2.74/month)	\$	26,304
STATE SHARED (1.5 residents/household)	\$110/resident	\$	132,000
LOCAL OPTION SALES TAX	\$150/resident	\$	180,000
TOTAL - RECURRING REVENUE (ANNUAL)		\$	1,030,986



November 3, 2013

TO: Board of Mayor and Aldermen

FROM: Eric Stuckey, City Administrator *Eric*
Vernon Gerth, Assistant City Administrator for Community & Economic Development
David Parker, CIP Executive/City Engineer
Paul Holzen, Director of Engineering Department
Catherine Powers, Director of Planning & Sustainability Department
Jonathan Langley, Long Range Planning Supervisor of Planning & Sustainability Department

SUBJECT: Draft Plan of Service - Rogers Farm

Purpose

The purpose of this memorandum is to provide the Board of Mayor and Aldermen (BOMA) with information regarding the Annexation and Plan of Service for the Rogers Farm.

Background

Pulte Group has requested annexation of a portion (~400 acres) of the Rogers Farm located within the northwest part of Franklin's Urban Growth Boundary (UGB) and adjoining the current city limits of Franklin. The Pulte Group proposes a 781 detached, single-family dwelling age restricted Del Webb community. The site is located in a rural section of Williamson County and the Plan of Service stipulates the infrastructure the applicant is required to provide to adequately support the project.

Plan of Services

City departments have provided feedback for the Plan of Services and their requirements are outlined in the attached draft. As expected, the Plan of Service primary issues are wastewater and roadway infrastructure. The following is a summary of these improvements:

Wastewater

Based on the West Drainage Basin Study, staff is recommending the following:

- The existing Westhaven Pump Station be relocated to the Rogers Farm and upgraded to handle sewer flow from the entire West Drainage Basin.
- A forty-two inch gravity sanitary sewer interceptor main be constructed to extend the Westhaven Interceptor to the relocated pump station.
- The new pump station will require a sanitary sewer force main to be constructed through the Rogers Farm Development and along Del Rio Pike to the existing fourteen inch (14") sanitary sewer force main in the general vicinity of the intersection of Del Rio Pike and Carlisle Lane and continued along the alignment (paralleling) of the fourteen inch (14") sanitary sewer force main to the City's Water Reclamation Facility on Claude Yates Drive.
- The developer would be responsible for engineering design, necessary easement and right-of-way acquisition, installation of the force main, and paying for construction of the pump station.

Note: In accordance with the City's Municipal Code, recovery is available for wastewater improvements that exceed the infrastructure required for serving the development. It should be expected the sanitary sewer improvements associated with this project will be phased to coincide with development and that developments agreements related to phasing and cost recovery would be presented to the Franklin Municipal



Planning Commission and BOMA for consideration concurrently with the Annexation, Rezoning, Development Plan, and Plan of Service.

Street Improvements

The most important aspect of street improvements required to serve this property is the project must have two means of ingress and egress. The proposed secondary means of egress is via existing roads within the Whitehall Subdivision. An alternative would be through the Gentry Farm which would require acquisition and construction of a new road. Other requirements include:

- The applicant has selected access scenario number 1 to assure two means of access. This scenario would require access off Del Rio Pike, south of Whitehall Drive. The second access would be provided by connecting Brinkley Drive and Whitehall Drive within the Whitehall Subdivision. If this scenario cannot be accomplished the City, with Board approval, would need to assist Pulte in condemning easements on the Gentry Farm property to provide the second access.
- Carlisle Lane and the north-south portion of Del Rio Pike are identified in a future major collector. The developer is required to construct a Major Collector section from Highway 96 to the Intersection of Del Rio Pike and Cotton Lane. Specifically, the developer shall provide two twelve-foot drive lanes and a sixteen-foot median, two bicycle lanes, and sidewalks.

Staff has indicated that these improvements are vital to serve this project, even though the improvements will also assist future development.

The street improvements are related primarily to safety for the anticipated dwelling and can be phased, however staff believes the median is essential to the safety concerns and should not be phased. Since this is a collector, recovery is not an option.

Note: There are no off-sets available for the developer for the street improvements outlined. Currently, the City's Road Impact Fee ordinance only authorizes off-sets for improvements made to arterial roads.

Pulte Group – Initial Response to Plan of Service

City staff has met with the representatives from the Pulte Group regarding the draft Plan of Service. While there is agreement to the phasing of improvements to coincide with development, Pulte Group does not agree to the street improvements proposed by staff and have offered an alternative. Attached is a Plan of Service that includes changes (highlighted in red) as proposed by the Pulte Group.

Financial Impact

Included separately, staff has prepared a rough estimate of the revenues likely to be generated from the development of this property. This estimate is based on 800 homes with an average value of \$250,000 and does not include an estimate of the annual cost of the city providing service to this property when fully developed.

Next Steps

As discussed previously, staff recommends a Public Hearing be scheduled during the December 10, 2013 BOMA meeting to obtain additional input from citizens, the applicant, and staff. Following the public hearing, it is anticipated that the BOMA will either refer the Plan of Service back to staff, with or without comments, to begin working with the applicant on the Annexation, Rezoning, Development Plan, Plan of Service, and agreements or, the BOMA may decide, by majority vote, not proceed with annexing the property at this time. It should be noted that direction by the Board to refer the Plan of Service into the annexation process does not imply approval by the Board during later stages in the process. The referral simply moves the item for further work and review by staff, the FMPC and, ultimately, the Board.



City of Franklin Land Use Plan

The Rogers Farm is located within the West Harpeth Character Area - Special Area 3. The following is an excerpt from the City of Franklin Land Use Plan which supports residential development in this area.

WESTHARPETH PLANNING BACKGROUND

The West Harpeth Character Area contains 6,120 acres located on the northwestern edge of the UGB. New Highway 96 West transverses the area, and Hillsboro Road is near the eastern boundary. The area is bound to the northeast by the Harpeth River and to the west and northwest by the West Harpeth River. The Central Franklin Character Area is to the east, and the Southall Character Area bounds the area to the south.

About one-half of the land in the area is vacant or agricultural. The developed land is primarily residential, although nonresidential uses have been approved in conjunction with the Westhaven development. The Harpeth and West Harpeth Rivers create substantial floodplain constraints. The area is also home to historic farmstead sites.

This area is in the midst of rapid change. Major planned developments are in the process of approval and construction. The Westhaven TND is the dominant new development for the area. There will be some 2,700 dwelling units provided by this and other approved developments. These developments are taking place in comprehensive mixed-use designs, with a variety of housing types integrated with Neighborhood and Local Retail, office spaces and community open space and facilities.

The forces that may have the greatest impact on the future character of the West Harpeth Character Area include the improvement of infrastructure. Both the completion of Mack Hatcher Parkway and expanded power lines will open the area to additional development. A sewer line is being constructed to handle the future needs of the area.

While property rights are respected, new development should be timed with availability of infrastructure. The subdivision of land into one-acre lots proliferates sprawl and is inappropriate in this area.

Vision

The West Harpeth area will be an exemplary model for the way in which new development can be accommodated while preserving open space, sensitive environmental features and historic farmsteads. It will be an area of high quality guided by design standards and mixed-use developments.

Special Area 3

1. This area is appropriate for a Hamlet, a Conservation Subdivision or a TND pursuant to a Master Plan and that at least fifty percent of the area—targeted towards environmentally sensitive or scenic areas and vistas—is preserved as permanent open space.
2. A 300-foot setback is recommended for the north side of New Highway 96 West.
3. Commercial, Office and Retail uses shall not front along New Highway 96 West.